



A presentation for the

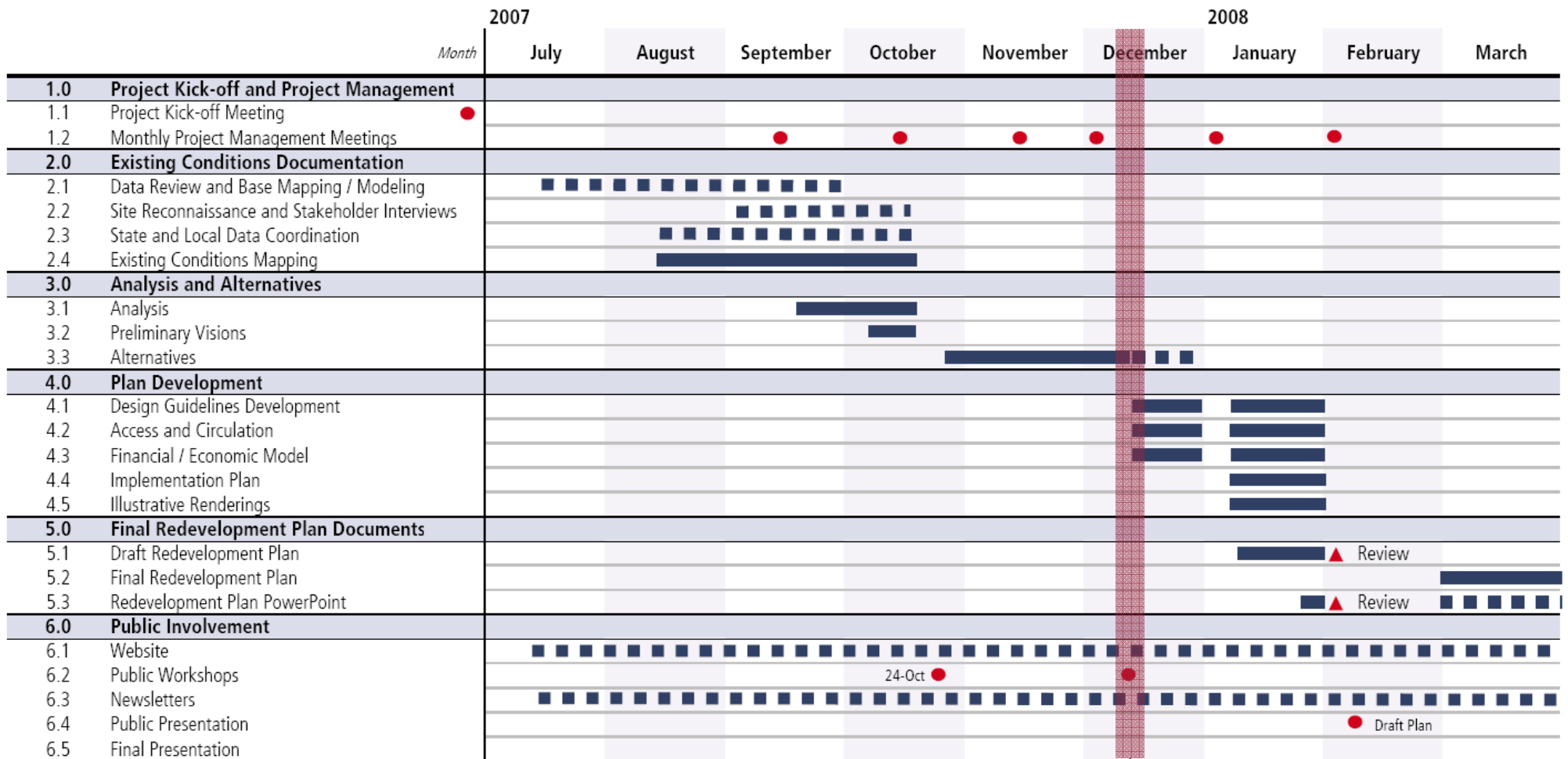
Millburn Downtown Redevelopment Plan

January 24 and 29, 2008

Designated Redevelopment Areas

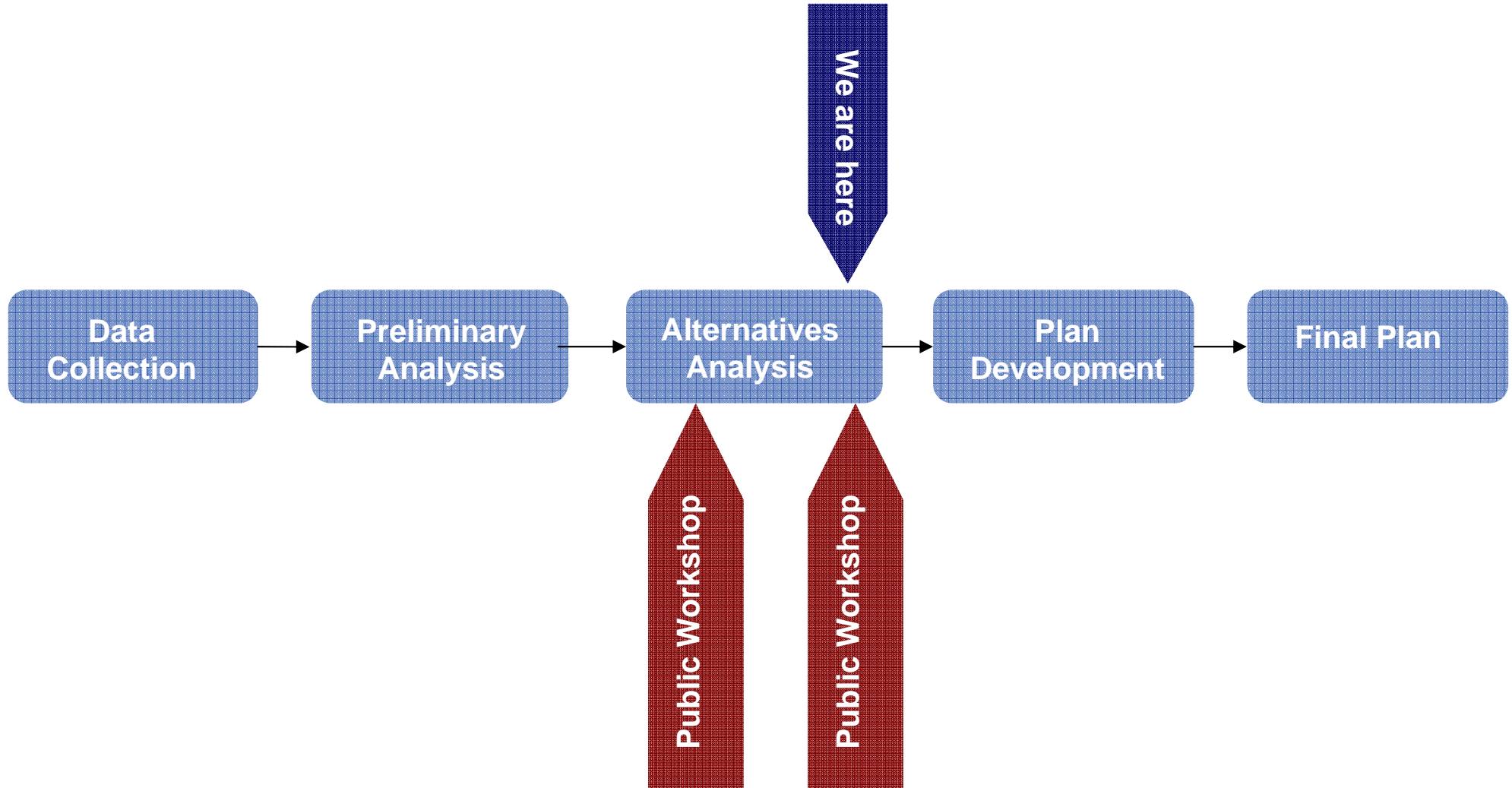



Project Schedule



We were here, when it snowed

Process Diagram



An aerial architectural rendering of a city block. The scene shows a grid of streets with various building footprints and heights. The buildings are rendered in a light, semi-transparent style, allowing the street layout and surrounding greenery to be visible. The streets are labeled: 'Millburn Avenue' runs vertically on the left side, and 'Essex Street' runs horizontally at the top. The overall aesthetic is clean and modern, typical of urban planning visualizations.

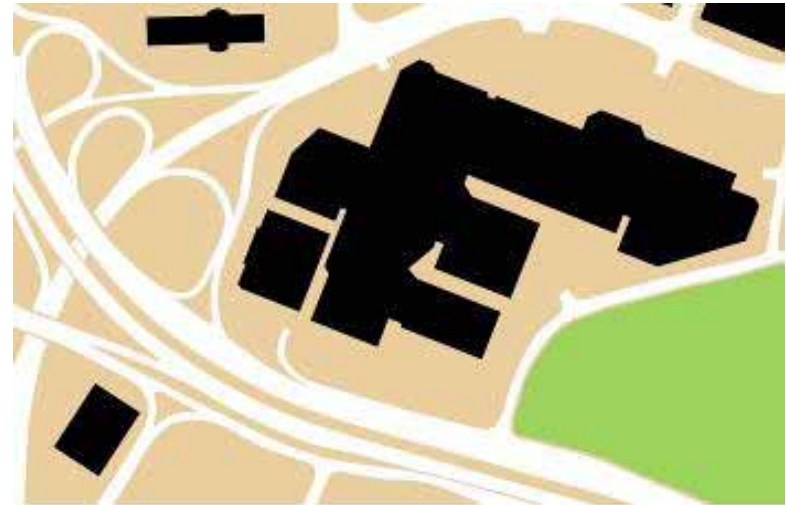
The goal of this Public Workshop is to review and comment on alternative concepts.

Scale Comparison

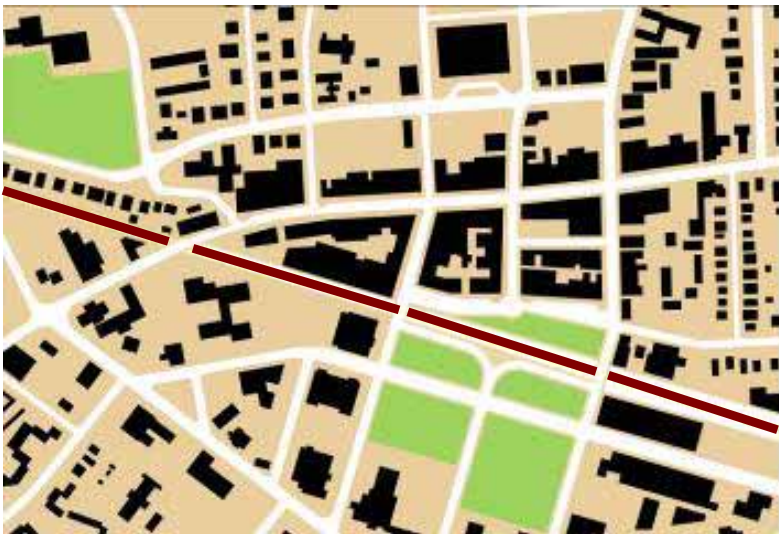
Madison



Short Hills Mall



Summit



Millburn



0 100 200 300 feet

What we heard at the public workshop



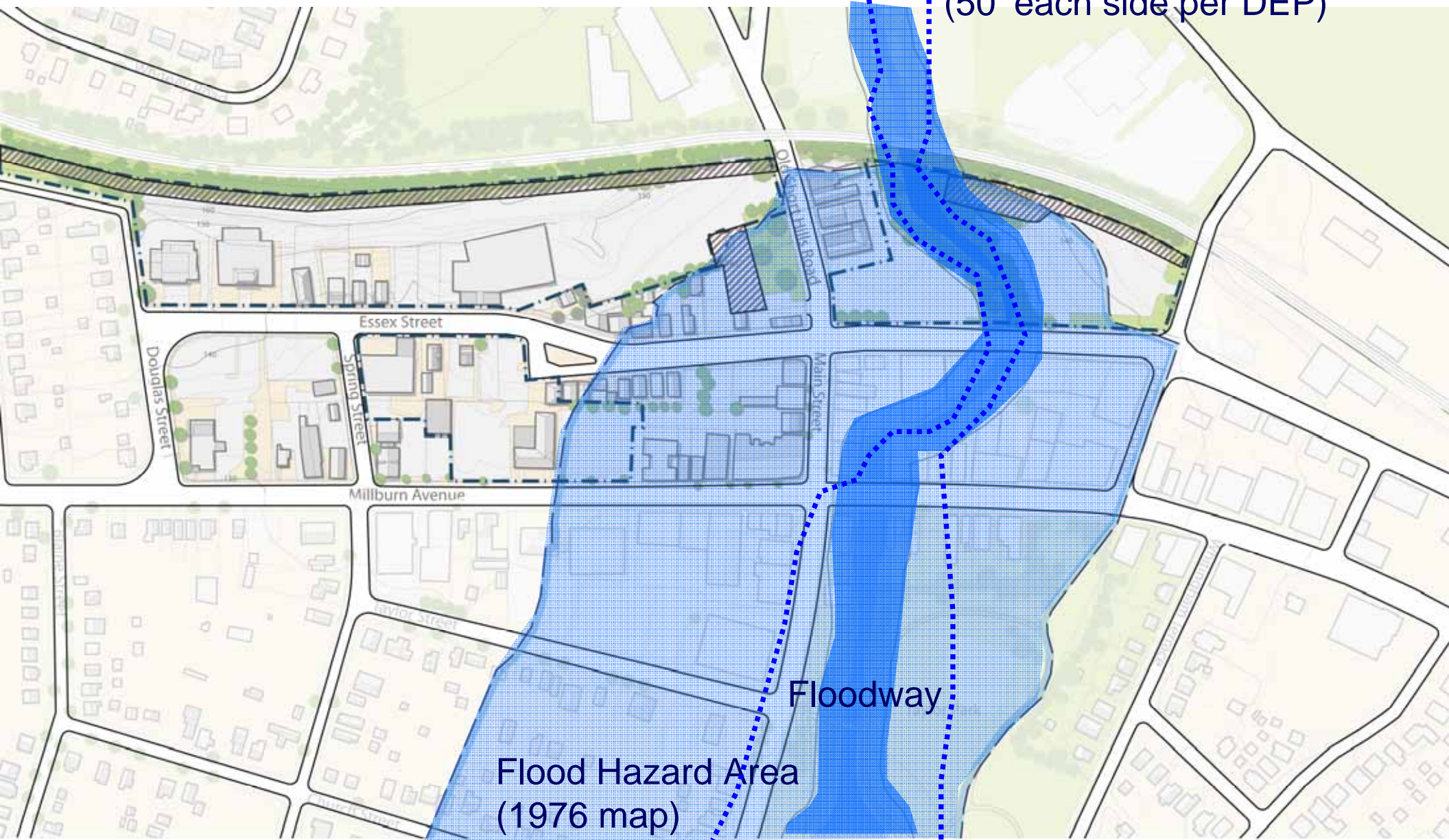
- There are concerns about traffic impacts.
- Mixed use development with retail and amenities is desired.
- The character should match downtown Millburn.
- The height should be no more than five stories.
- Open space along the river is a good idea.
- Potential uses include bookstores, grocer, recreation center/gym,

Overarching Issues

- Floodplain
- Parking
- Relocation of the Department of Public Works Yard
- Traffic Congestion

Floodplain

Riparian Buffer
(50' each side per DEP)



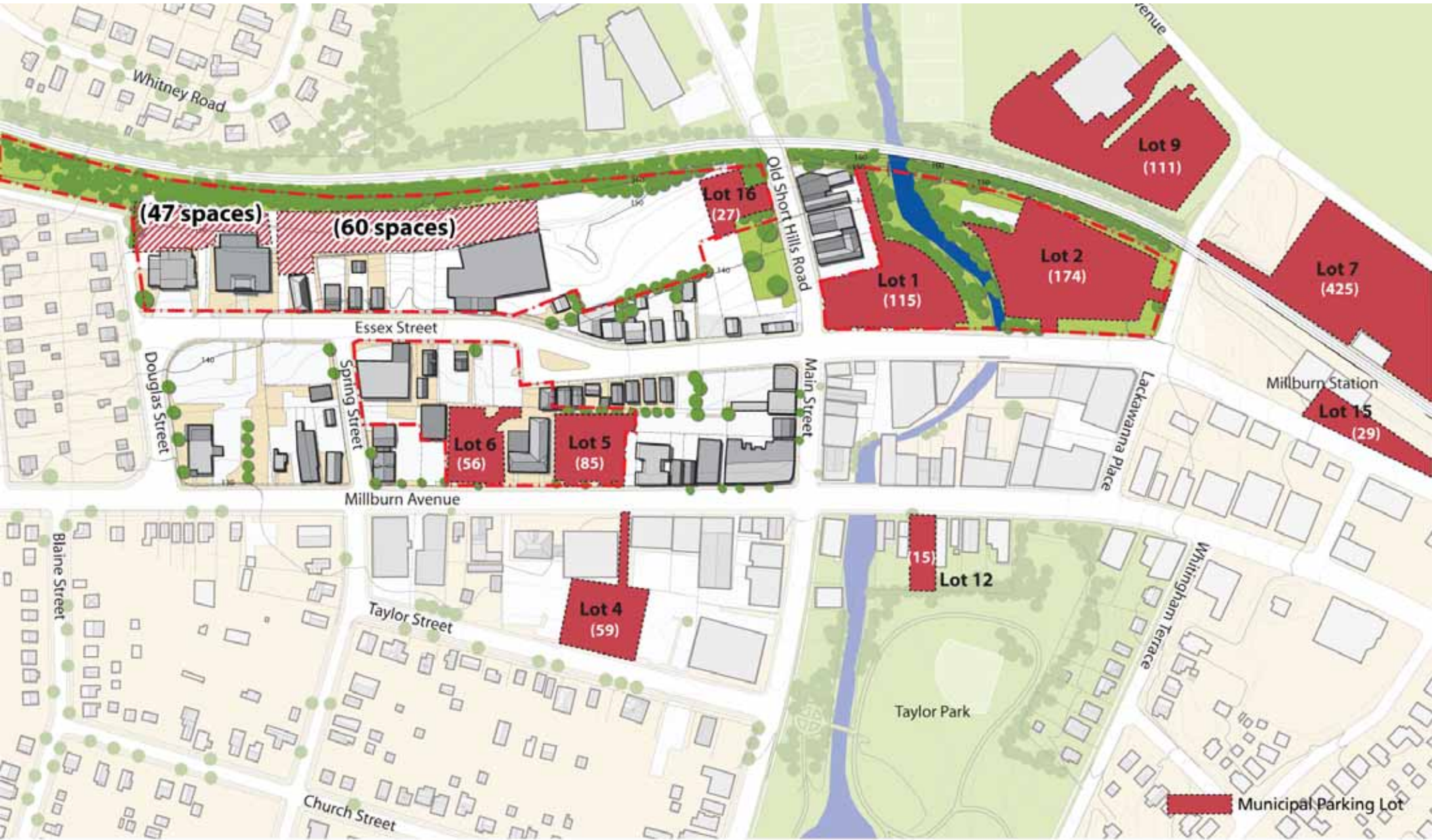
Floodplain Strategy

- The area that is not developable could be used as public open space.
- Lowering the area along the river could increase flood storage and lower the overall floodplain.
- Working with NJ DEP, lowering the area along the river could allow for raising other areas out of the floodplain.

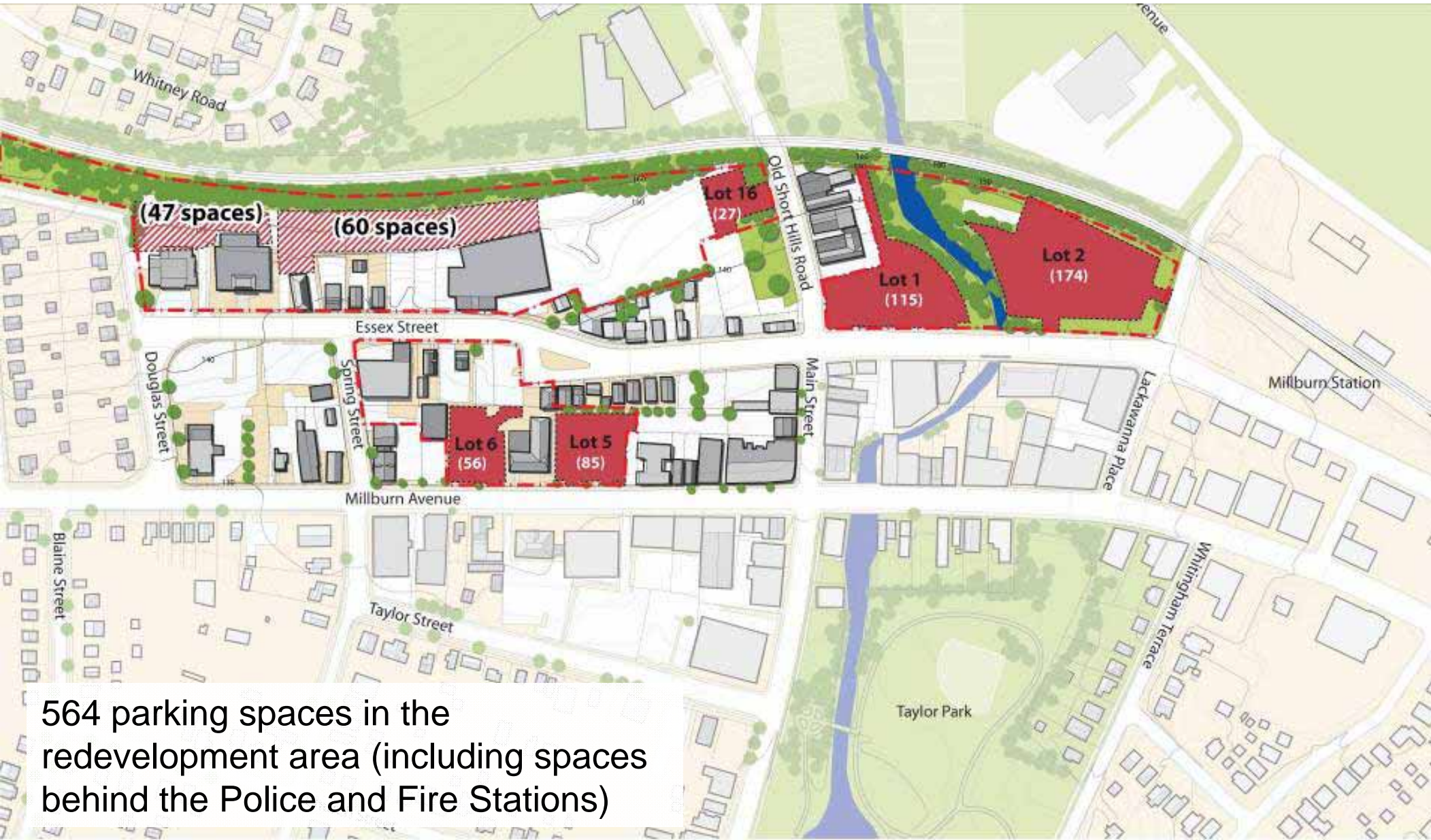
Floodplain Strategy



Parking



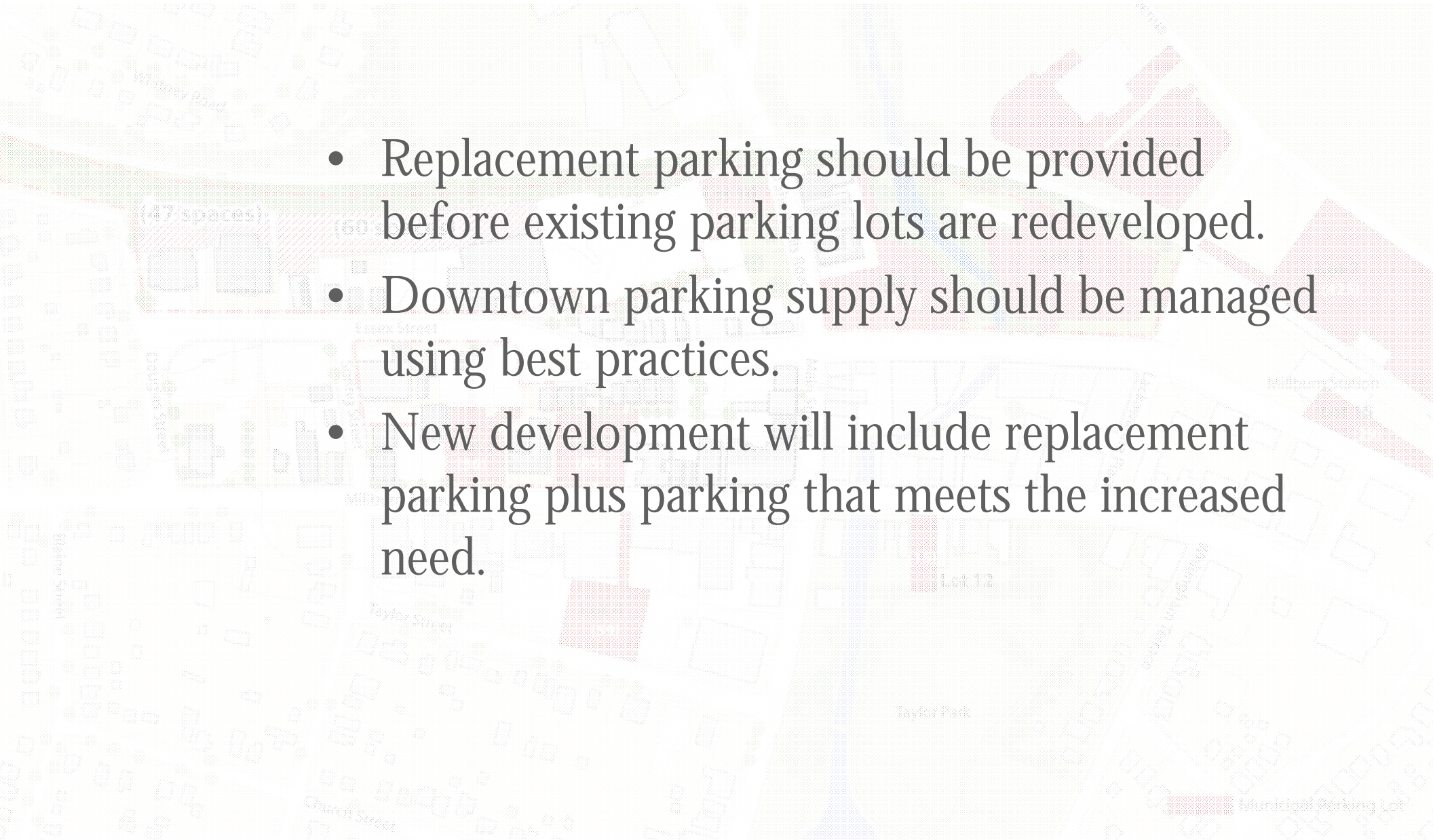
Parking



564 parking spaces in the redevelopment area (including spaces behind the Police and Fire Stations)

Parking Strategy

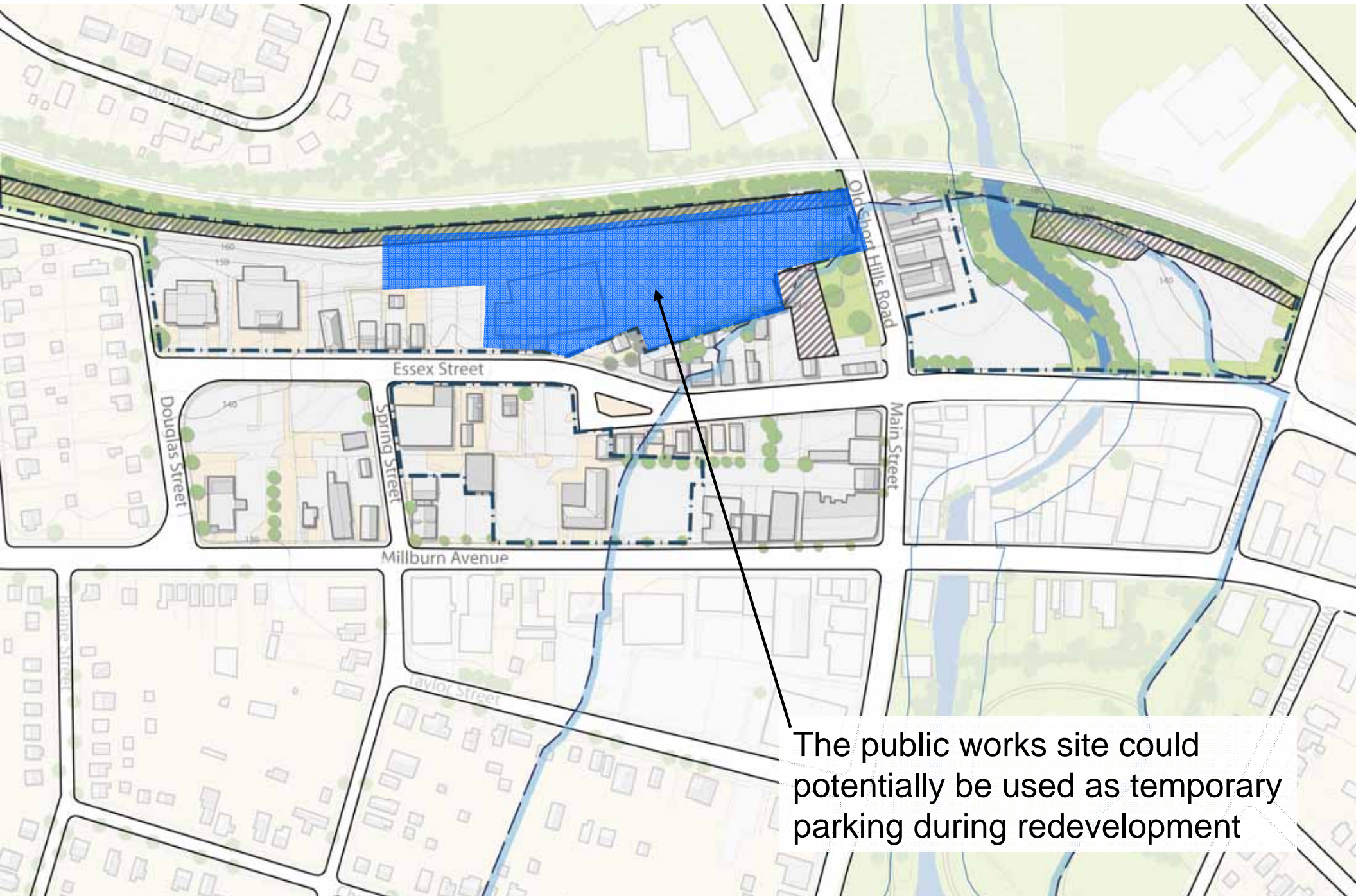
- Replacement parking should be provided before existing parking lots are redeveloped.
- Downtown parking supply should be managed using best practices.
- New development will include replacement parking plus parking that meets the increased need.



Public Works Site



Public Works Site



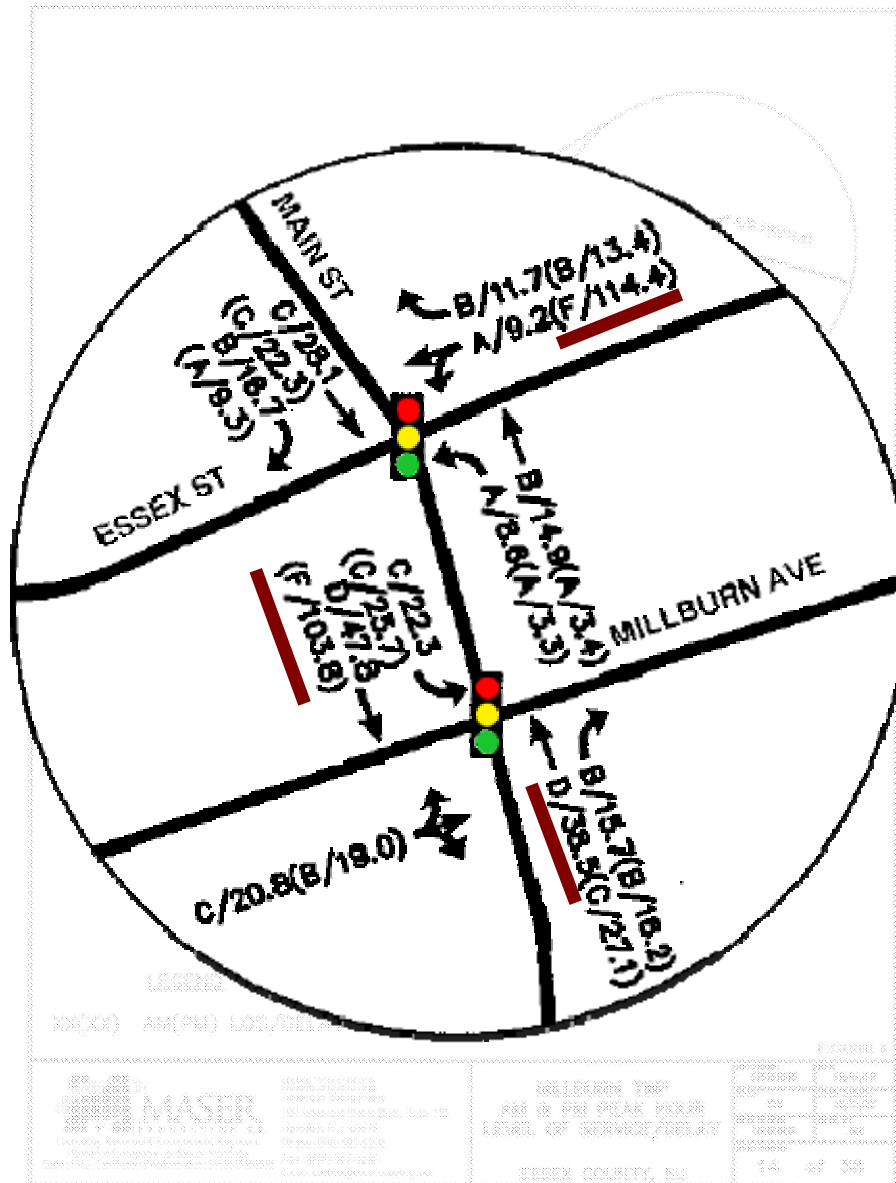
The public works site could potentially be used as temporary parking during redevelopment

Traffic Congestion



The public wants to know what the traffic impacts of redevelopment are.

Traffic Congestion

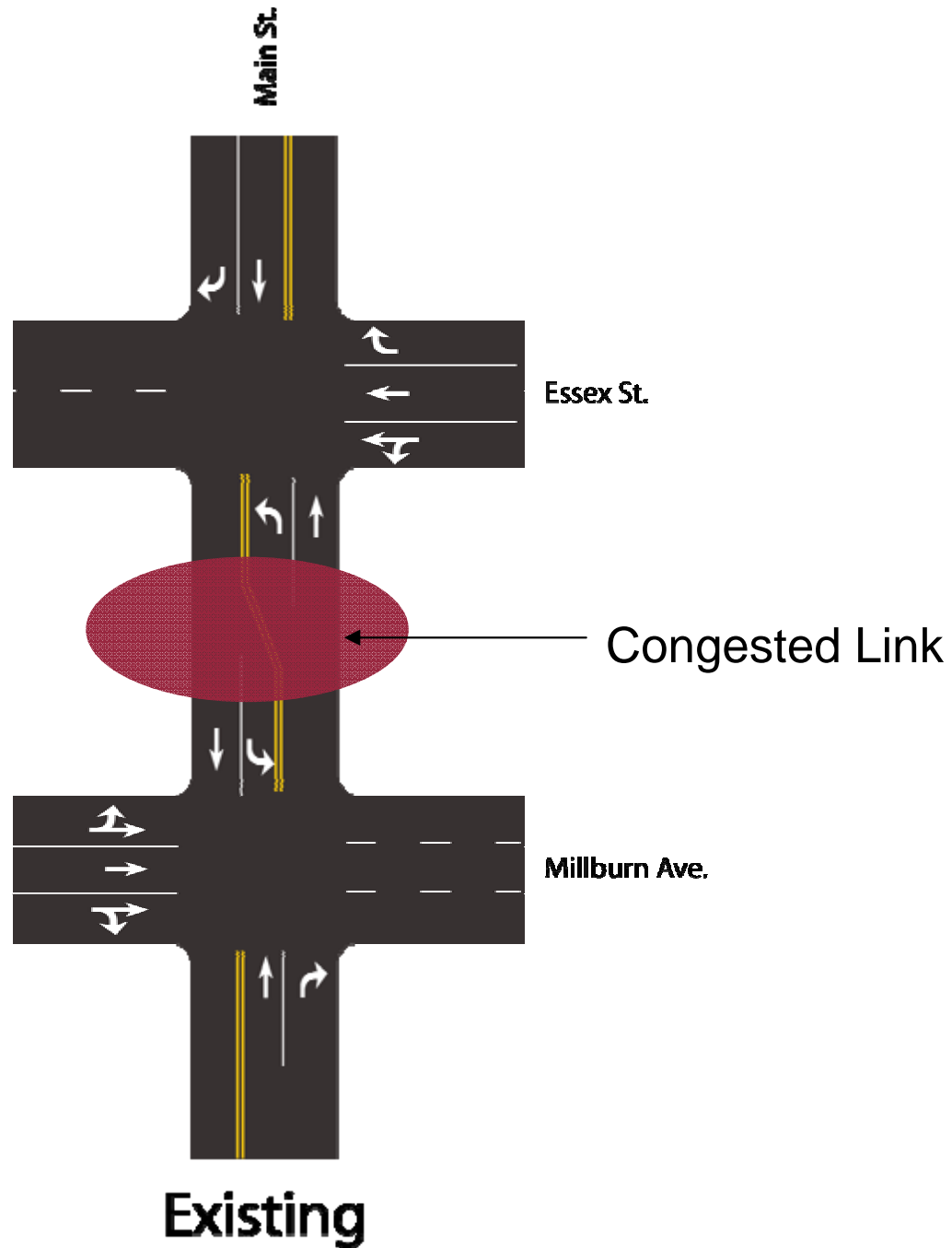


The current traffic study has analyzed prohibiting turns in order to decrease congestion.

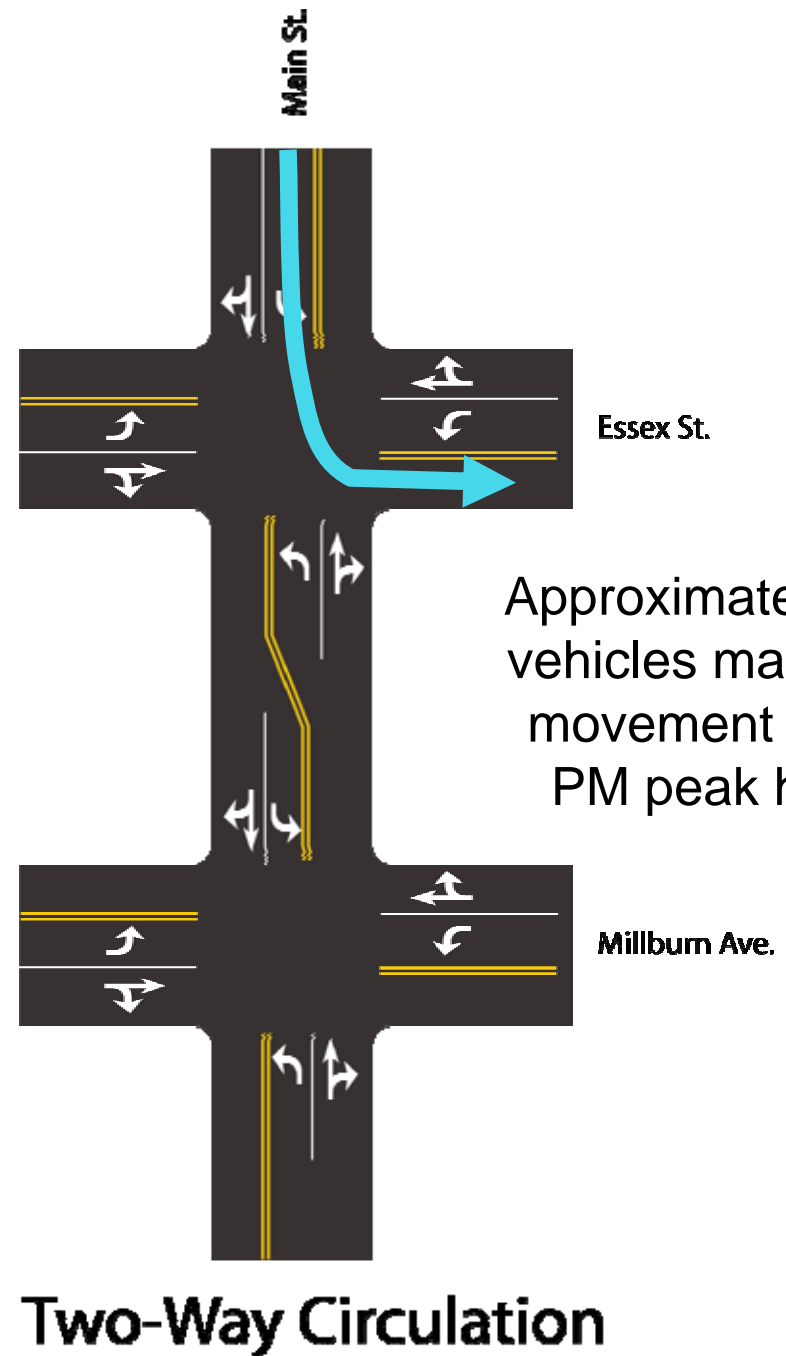
An alternative approach:

- Convert Essex and Millburn to two way streets.
- Create a more connected street grid.

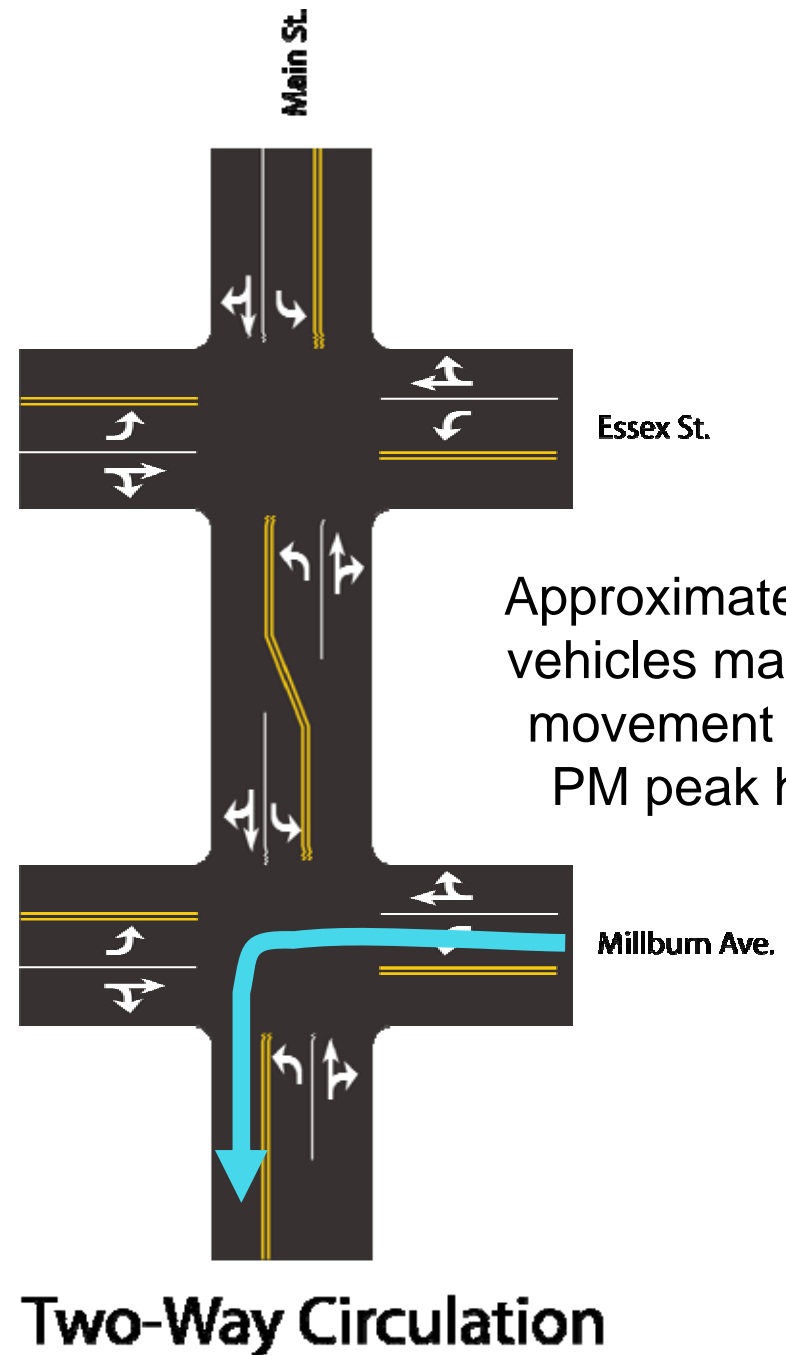
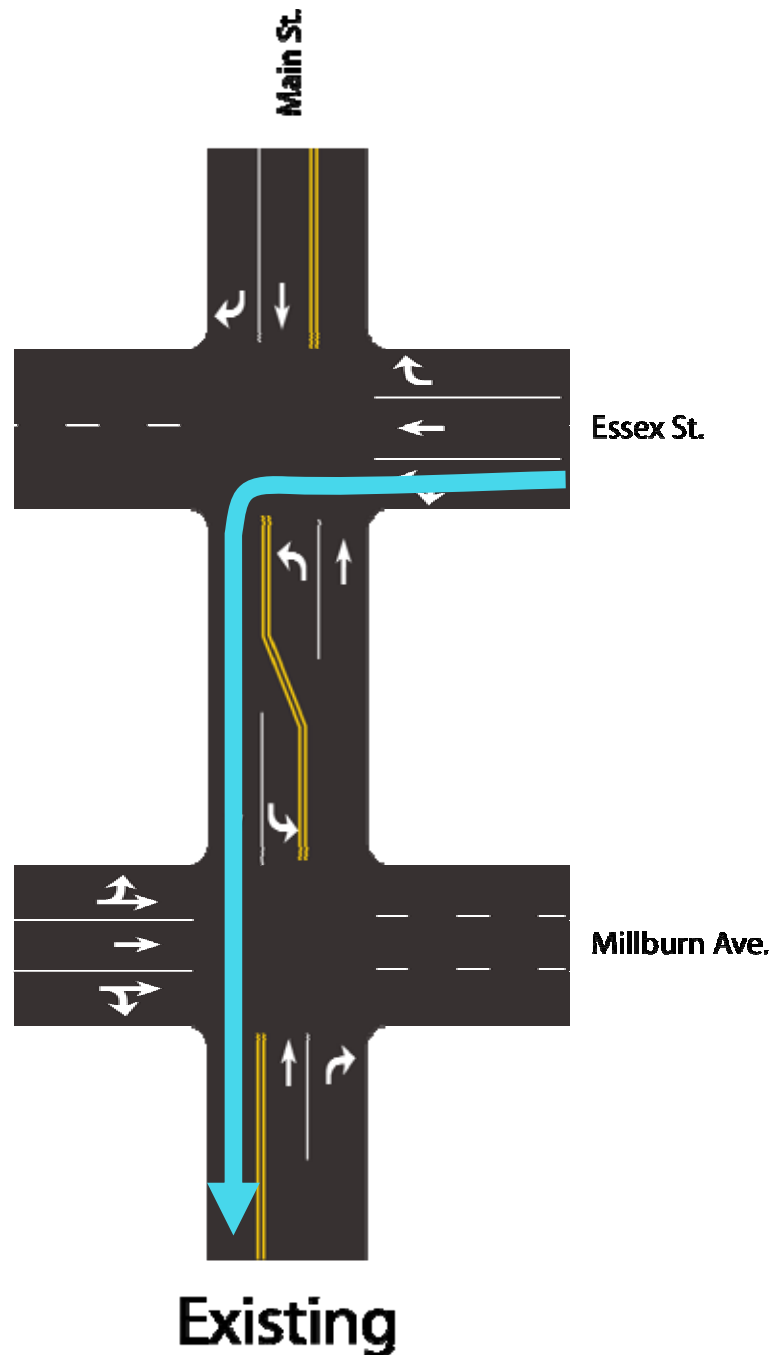
Converting One-Way Streets to Two-Way Streets



Converting One-Way Streets to Two-Way Streets




Converting One-Way Streets to Two-Way Streets



Converting One-Way Streets to Two-Way Streets



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Many cities changing one-way streets back

Updated 12/20/2006 10:43 PM ET

By **Melanie Eversley, USA TODAY**

More traffic will be coming to downtown Danville, Ill. — and that's how Danville wants it. The city of 33,000 is converting some of its longtime one-way streets back to two-way thoroughfares. City officials hope the change will make it easier for customers to reach downtown stores and shop in them.

"The driving force behind it is economic development," says city engineer David Schnelle, who expects to reprogram signals, change pavement markings and change signs by November 2007.

He says motorists tend to drive faster on one-way streets and go past their destinations, then lose time and patience backtracking.

Danville is one of hundreds of cities — from Berkeley, Calif., to Charleston, S.C. — switching one-way streets to two-way to improve commerce downtown, according to the American Planning Association

Numerous cities are converting one-way streets back to two-way operation in order to improve the environment of downtown and to improve traffic circulation.

Converting One-Way Streets to Two-Way Streets

One way streets reduce the visibility of some businesses, because some frontages become “eclipsed,” or hidden from motorists.

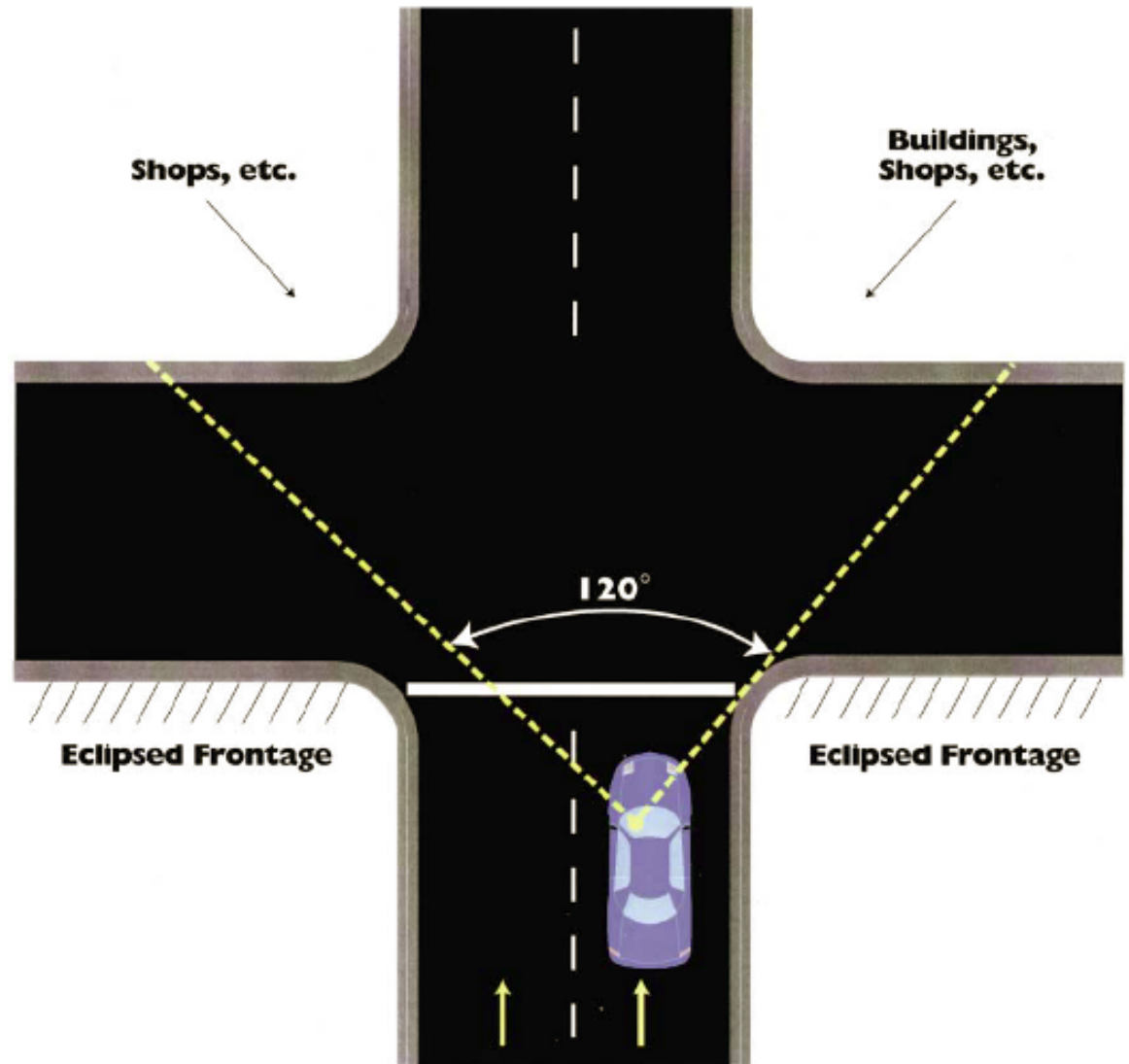


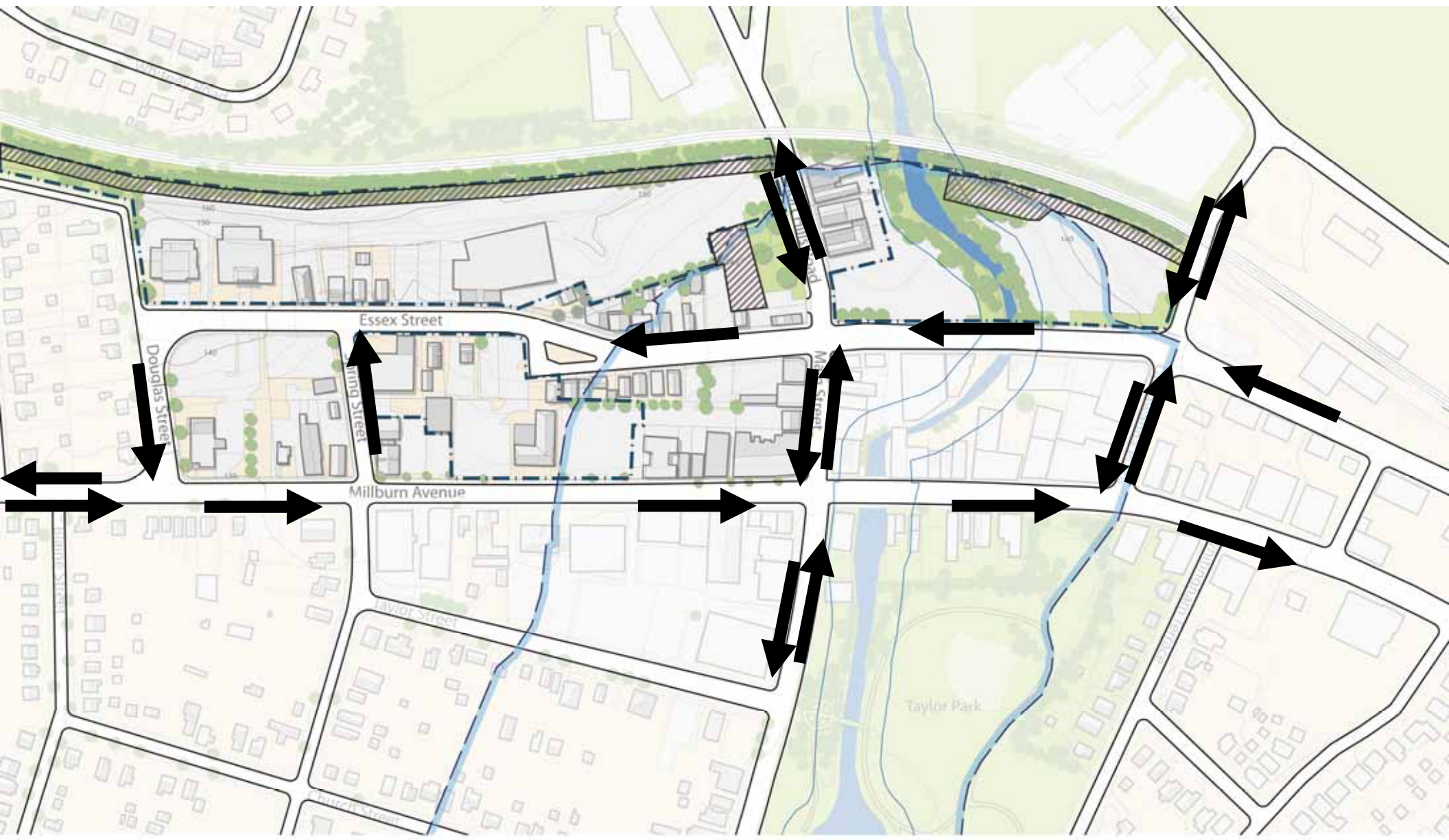
FIGURE 5 Retail eclipsing a diagrammatic summary.

Converting One-Way Streets to Two-Way Streets

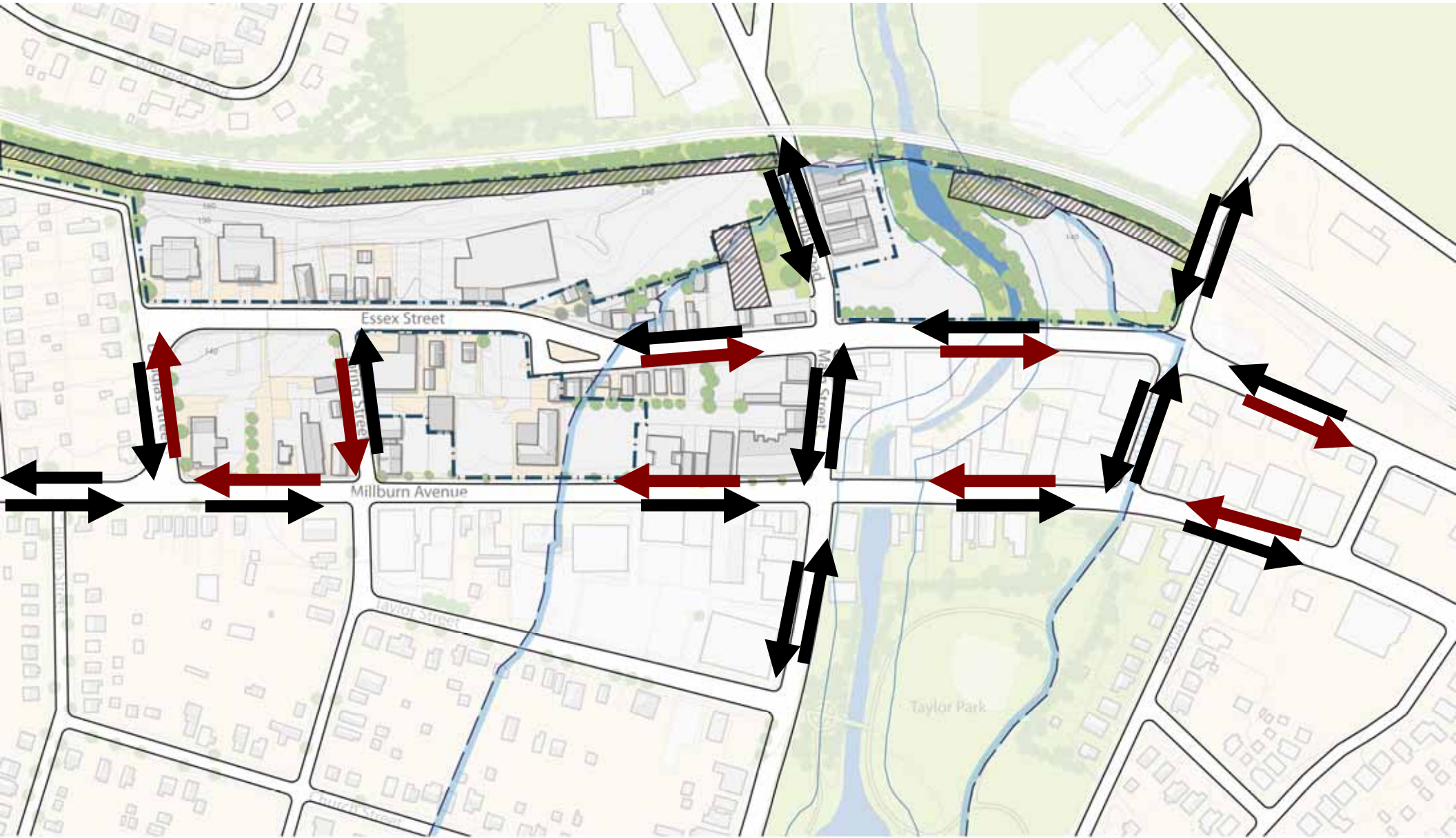
Conversion to two-way streets could have these benefits:

- Dispersing turning movements from congested intersections to less congested intersections.
- Simplifying circulation for downtown visitors.
- Improving the business climate.
- Reducing speeding and increasing walkability.

Existing Circulation

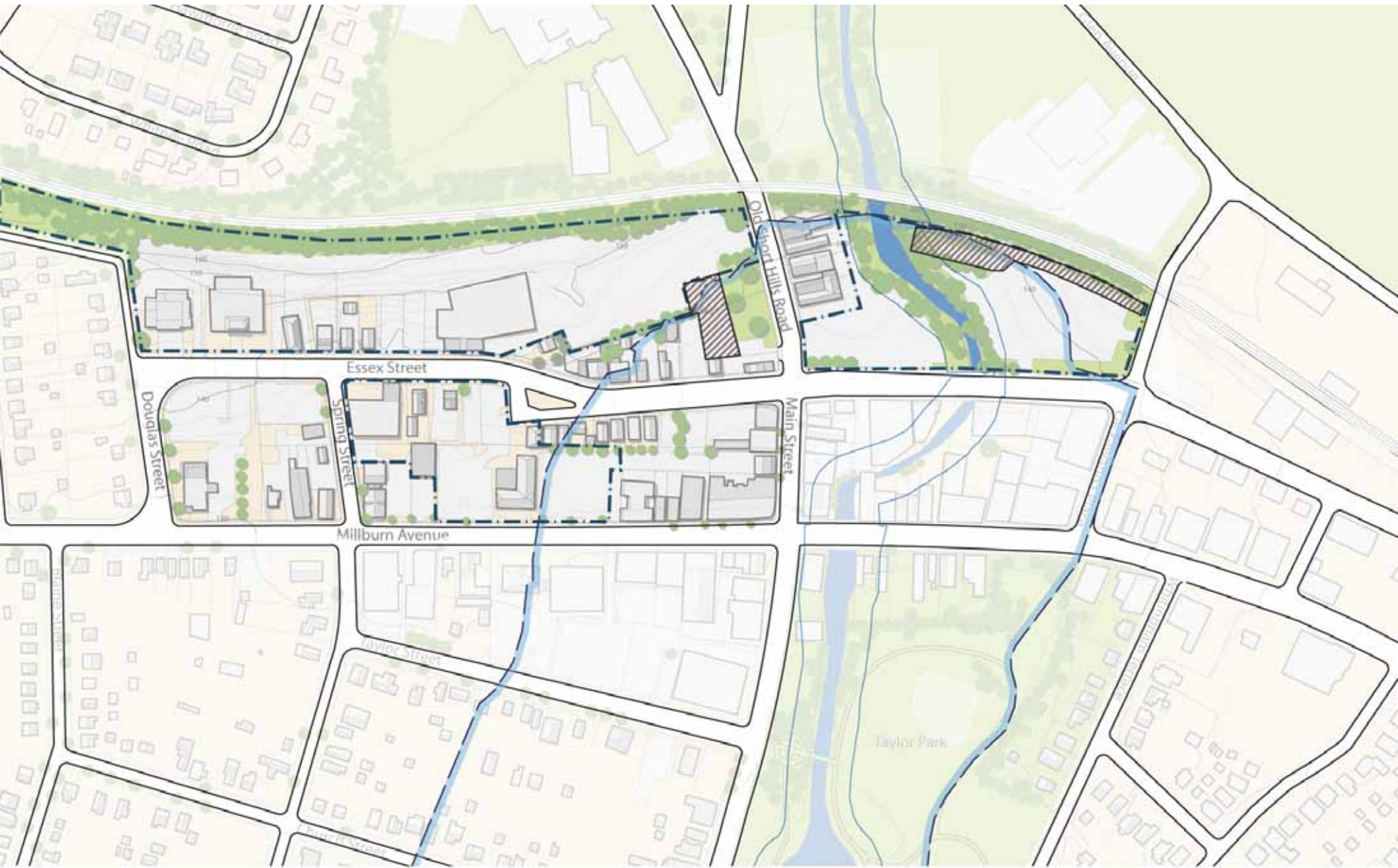


Two-Way Circulation





Creating a Grid of Streets



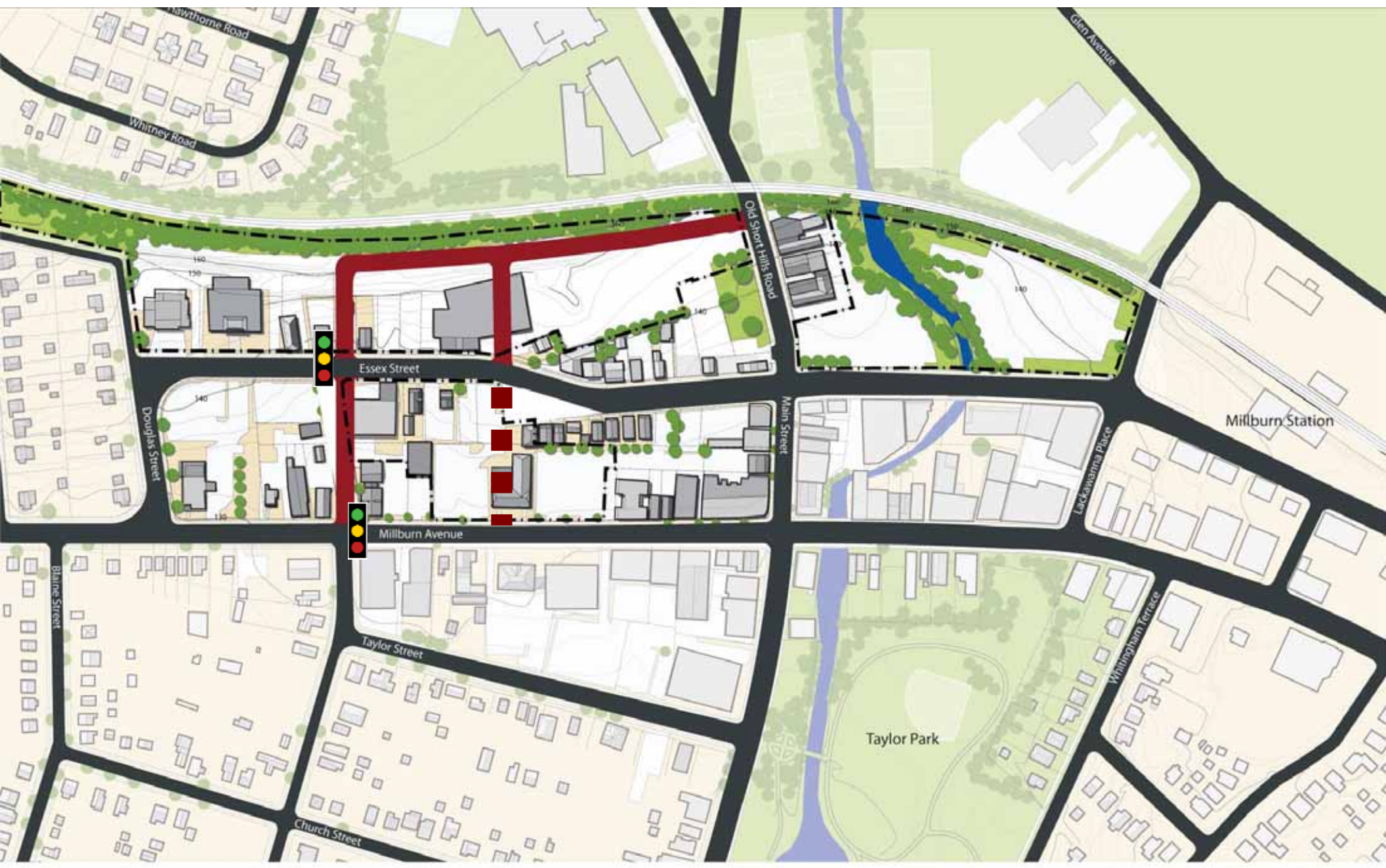
Existing Condition



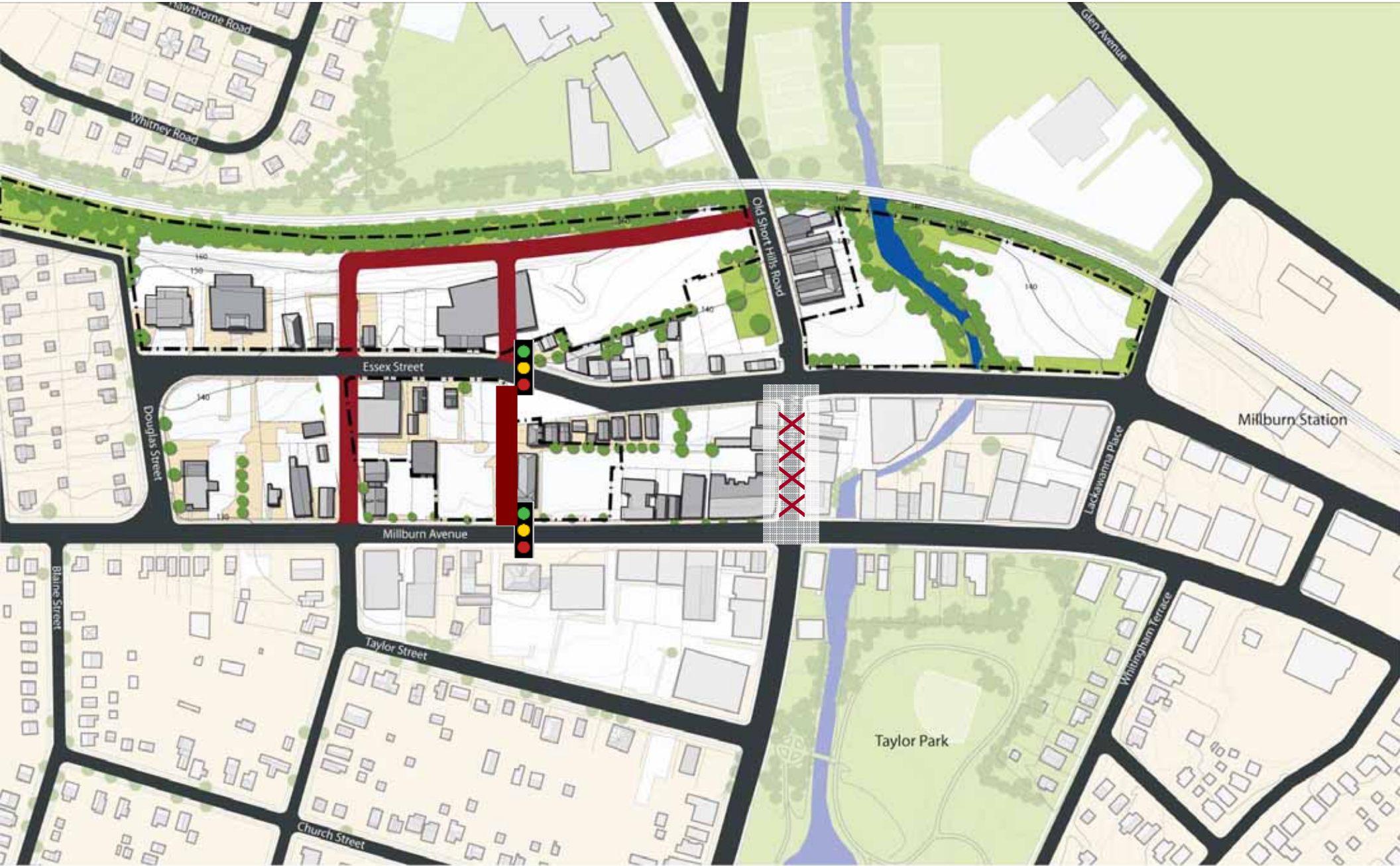
Option A: "Madison"

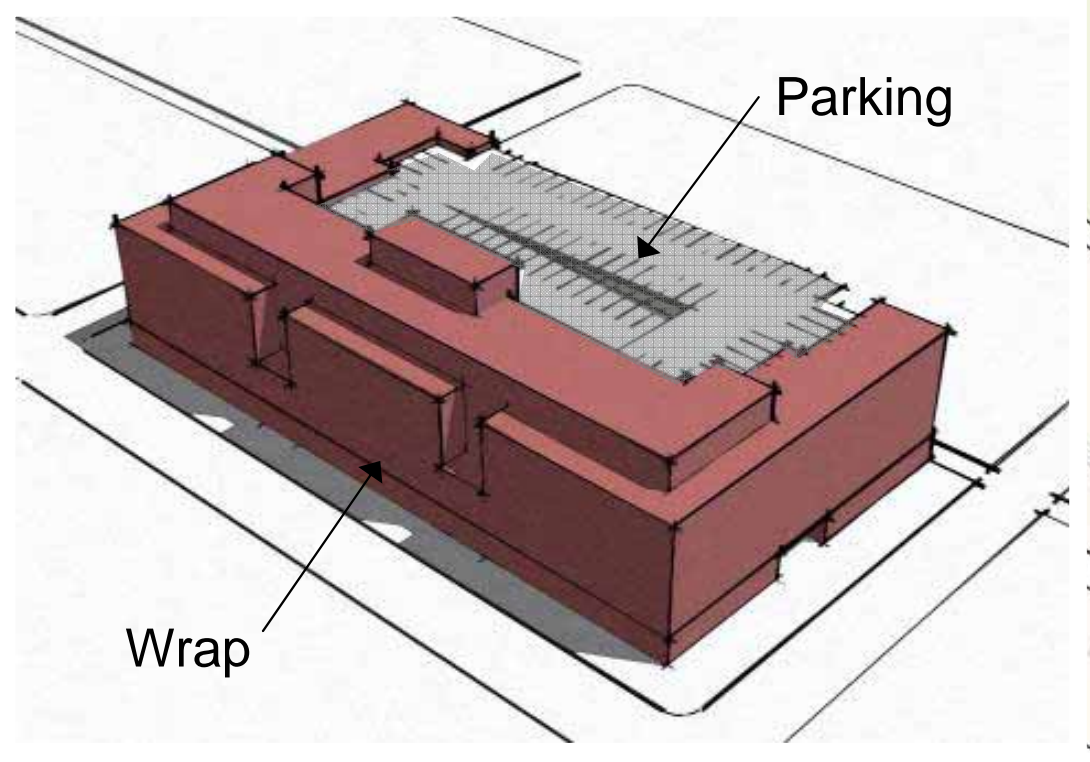


Option B: "Millburn Mews"



Option C: "Main Street Mall"





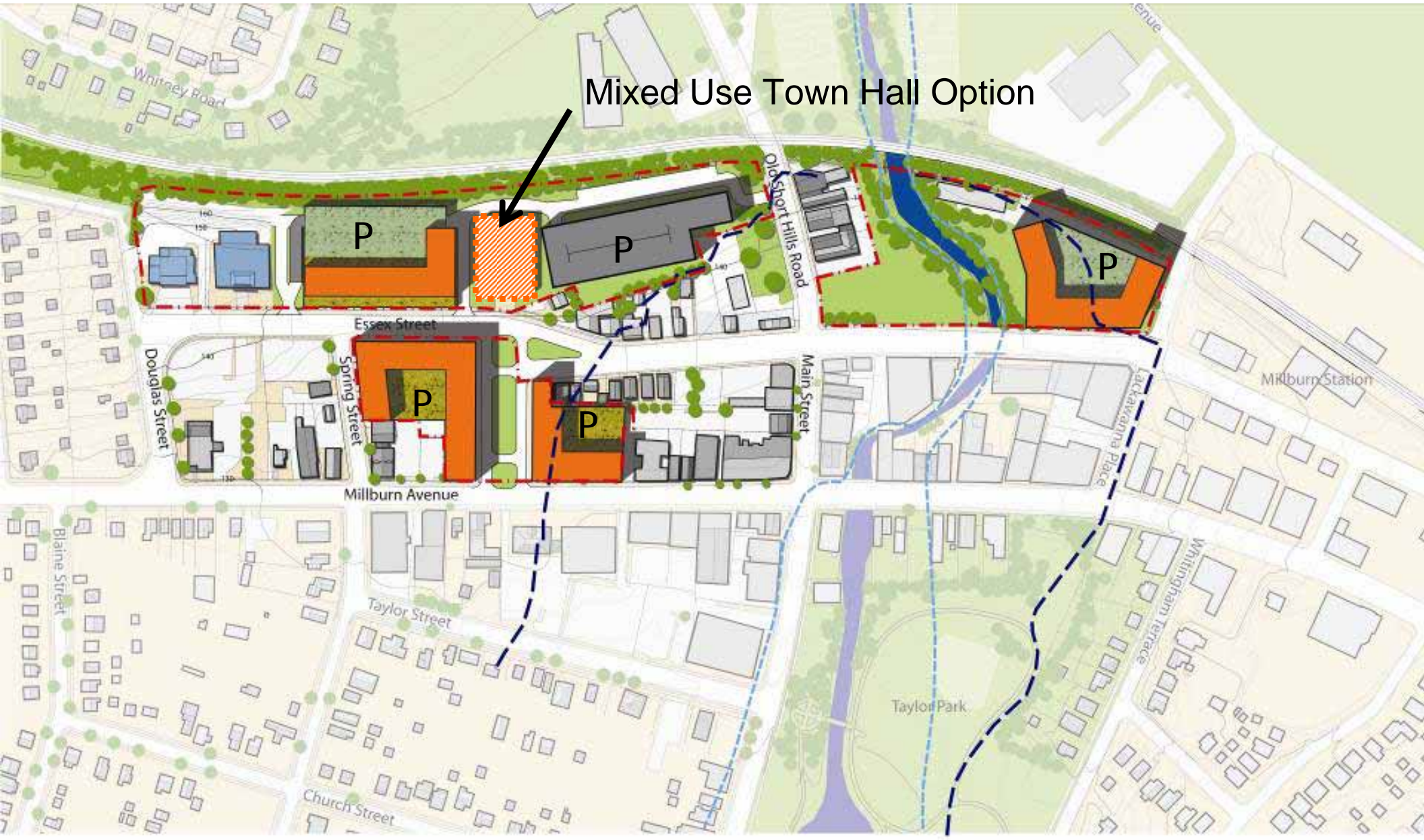
Garages can be wrapped in other uses.



Views from Downtown to the Mountain



Option A: "Madison"

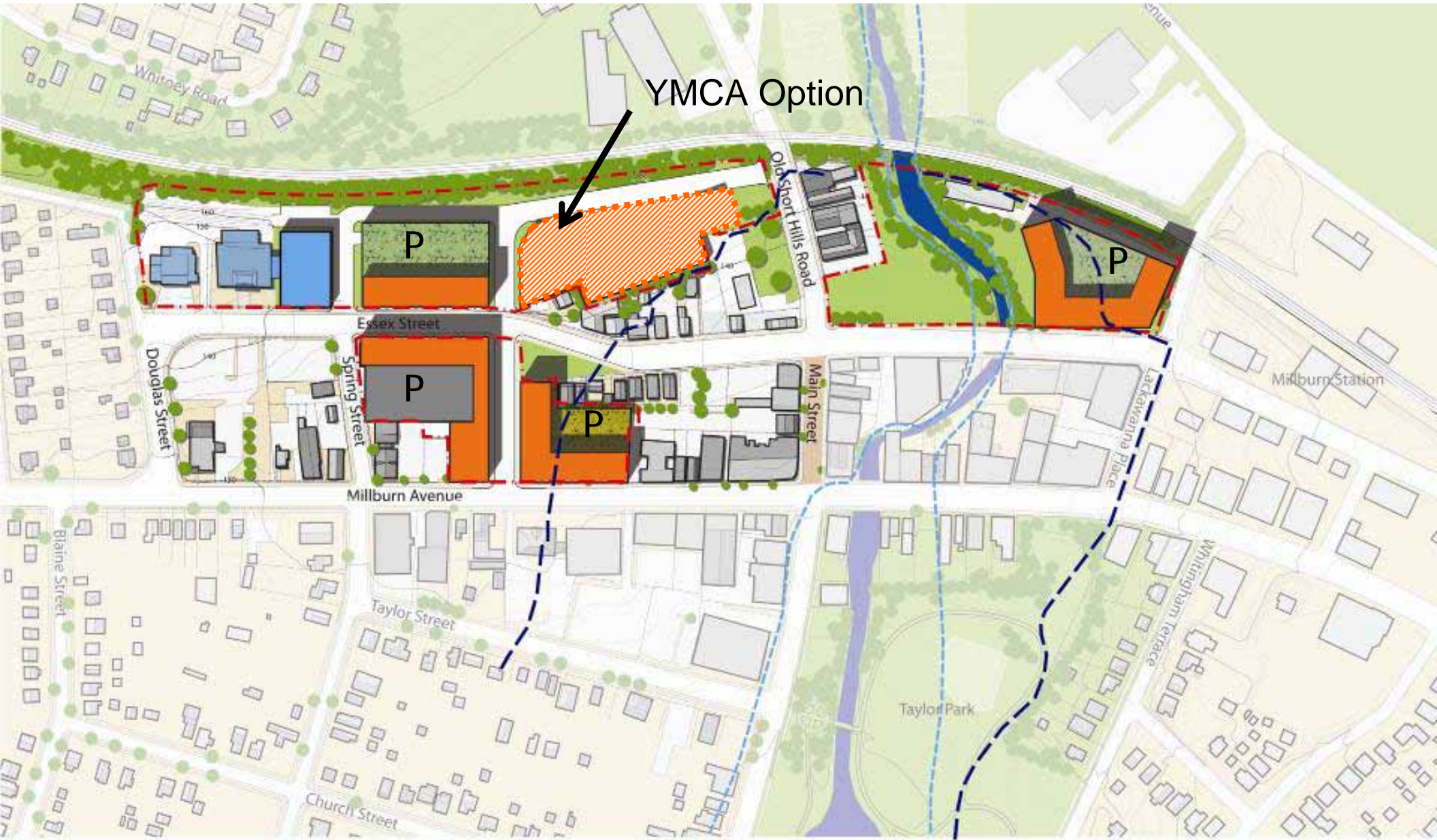


Mixed Use Town Hall Option

Option B: "Millburn Mews"



Option C: "Main Street Mall"



Program Summary

	Madison	Millburn Mews	Main Street Mall
New Residential	379 du	370 du	235 du
New Residential Parking (1.35~1.61/du)	515 spaces	595 spaces	369 spaces
New Commercial	135,160 gsf	137,665 gsf	122,821 gsf
New Commercial Parking (3/1,000gsf)	405 spaces	413 spaces	368 spaces
New Office	0 gsf	0 gsf	82,846 gsf
New Office Parking (3/1,000gsf)	0 spaces	0 spaces	249 spaces
New Town Hall	30,000 gsf	30,000 gsf	30,000 gsf
New Municipal Parking	148 spaces	115 spaces	76 spaces
Replacement Parking	517 spaces	517 spaces	517 spaces
Total Parking	1,585 spaces	1,640 spaces	1,579 spaces

Existing Condition



Option A: "Madison"



Option B: "Millburn Mews"



Option C: "Main Street Mall"



Existing Condition



Option A: "Madison"



Option B: "Millburn Mews"



Option C: "Main Street Mall"



Sustainability Principles

- Promote compact mixed use developments
- Create a public realm that focuses on human interaction and safe pedestrian circulation
- Ensure sustainable design of buildings and streets to mitigate development impact on floodplain
- Share parking where possible among compatible uses

An aerial photograph of a residential neighborhood, showing houses, streets, and green spaces, serving as a background for the text.

To Do List

- Meet with DEP regarding floodplain
- Traffic impact study
- Township to identify alternate site for DPW
- Prepare a preferred Redevelopment Plan
- Economic and fiscal study of proposed development

Questions for Tonight

- Should a two-way street system be considered?
- Where should a new Town Hall be located?
- Should additions be made to the street grid?
- Is the DPW site best used for a parking structure?
- Should additional parking (above replacement parking and parking relating to new development) be included?
- Should we consider converting one block of Main Street to a pedestrian mall?
- How should the site at the northeast corner of Essex and Main be used?
- Should a new street be added between Essex and Millburn?